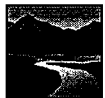


- * AS REQUIRED TO MATCH PROPORTIONATE CHANGE
- A NORMAL CENTERLINE GRADE
- B NOT TO EXCEED 0.5% IN EITHER DIRECTION
- GRADE BREAK GREATER THAN 1.0% MAY REQUIRE VERTICAL CURVE
- GRADE BREAK NOT TO EXCEED 1.0%

STREET TYPE	L
ARTERIAL	50'
PHASED ARTERIAL	50'
MAJOR COLLECTOR	40'
MINOR COLLECTOR	30'
INDUSTRIAL COLLECTOR	30'

DETAIL NO.

A1210



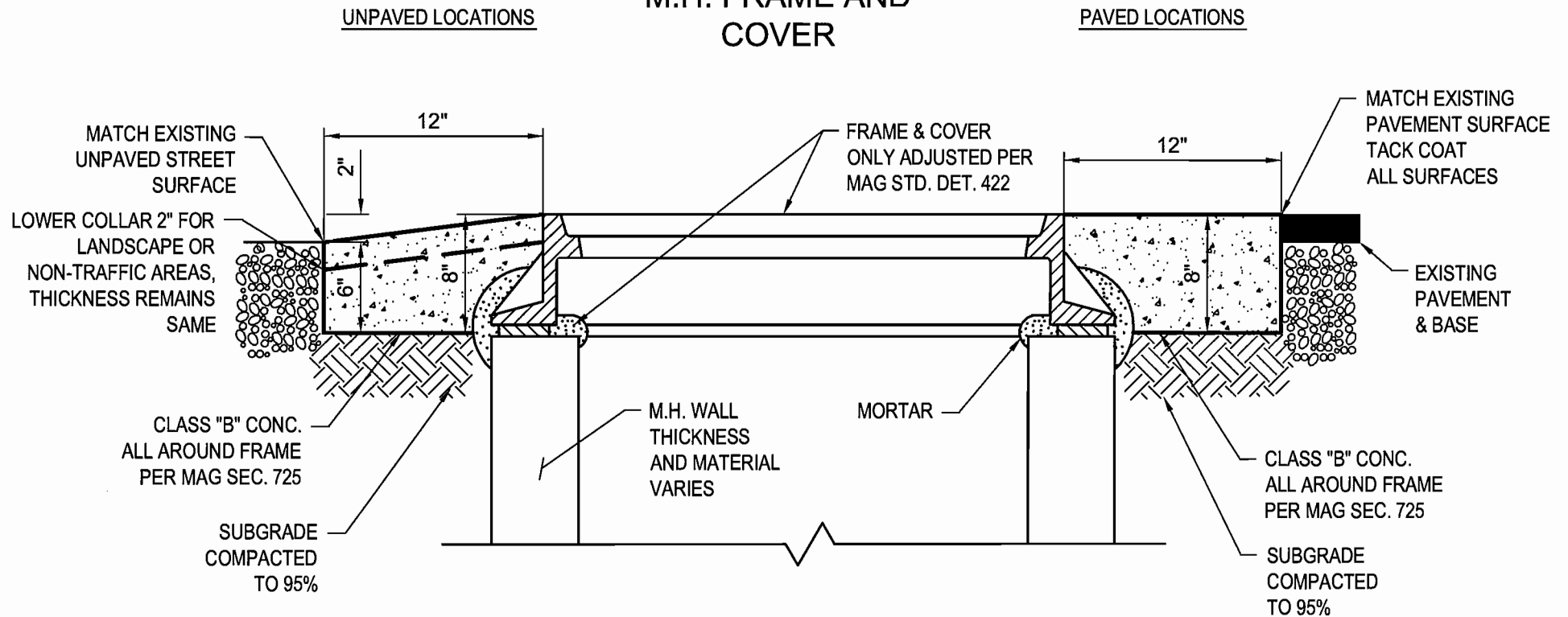
**CITY OF AVONDALE
STANDARD DETAIL**

INTERSECTION GRADING DETAIL

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08

M.H. FRAME AND COVER



NOTES:

1. THE ACCEPTABLE SURFACE PROFILE FROM THE PAVEMENT SURFACE ACROSS THE MANHOLE SHALL NOT VARY MORE THAN $\frac{1}{4}$ " FROM THE LOWER EDGE OF A 12 FOOT STRAIGHT EDGE WHEN THE STRAIGHT EDGE IS PLACED PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE ROADWAY.

DETAIL NO.

A1216



**CITY OF AVONDALE
STANDARD DETAIL**

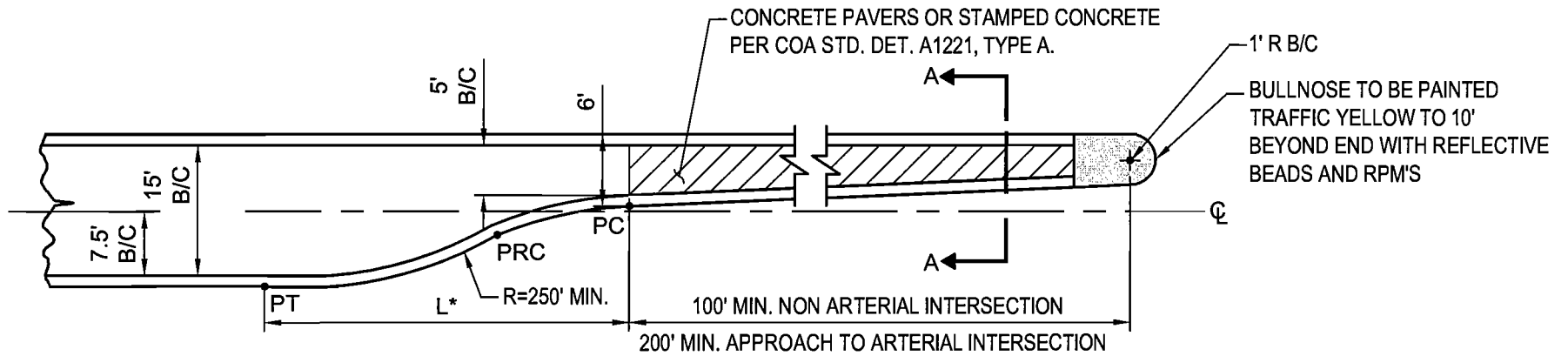
MANHOLE FRAME ADJUSTMENT

APPROVED BY:

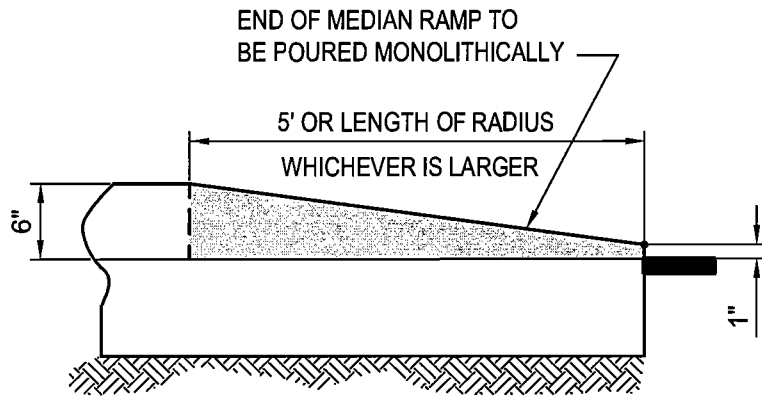
Daniel W. Fitzhugh

DATE:

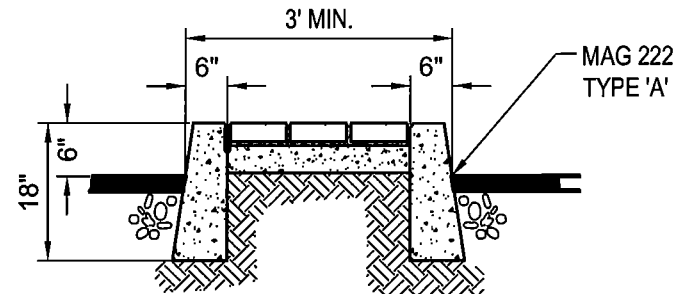
04-07-08



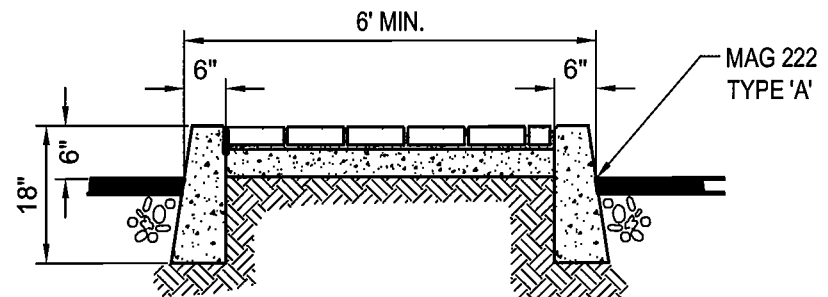
* TAPER LENGTH " L " = $WS/3$, WHERE " W " IS TURN LANE WIDTH IN FEET
AND " S " IS DESIGN SPEED IN MPH. MINIMUM " L " = 100'



END OF MEDIAN RAMP



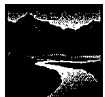
SECTION A-A



**SECTION A-A
(DUAL LEFT TURN LANES)**

DETAIL NO.

A1220



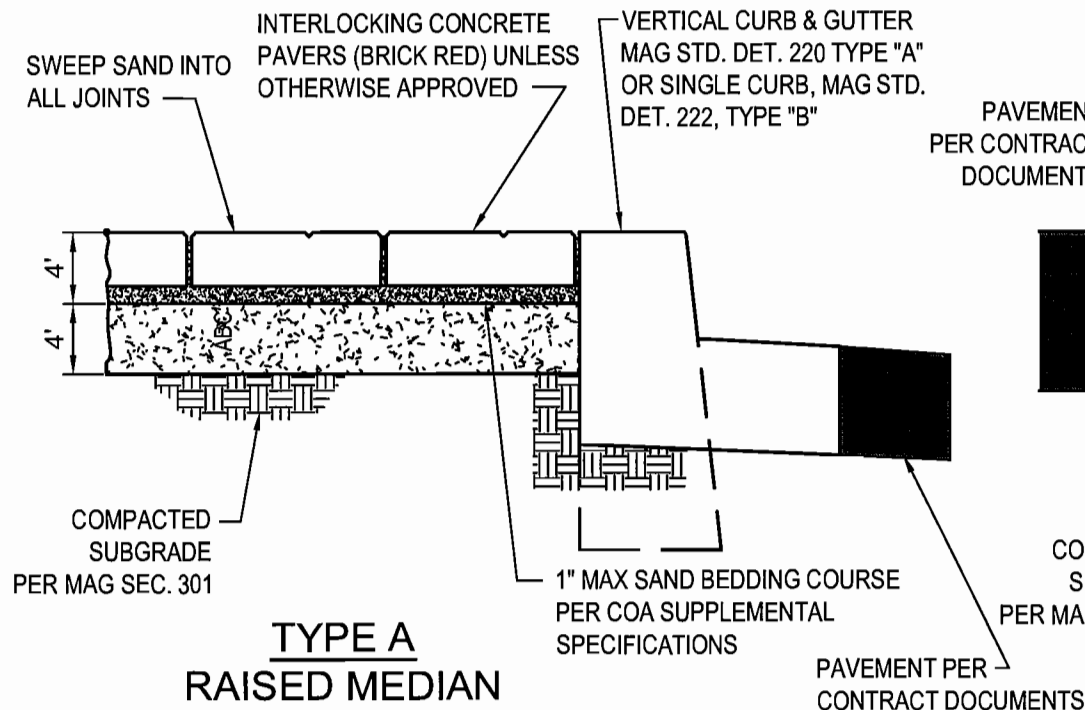
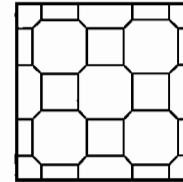
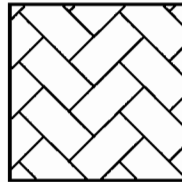
**CITY OF AVONDALE
STANDARD DETAIL**

GENERAL MEDIAN DESIGN

APPROVED BY:

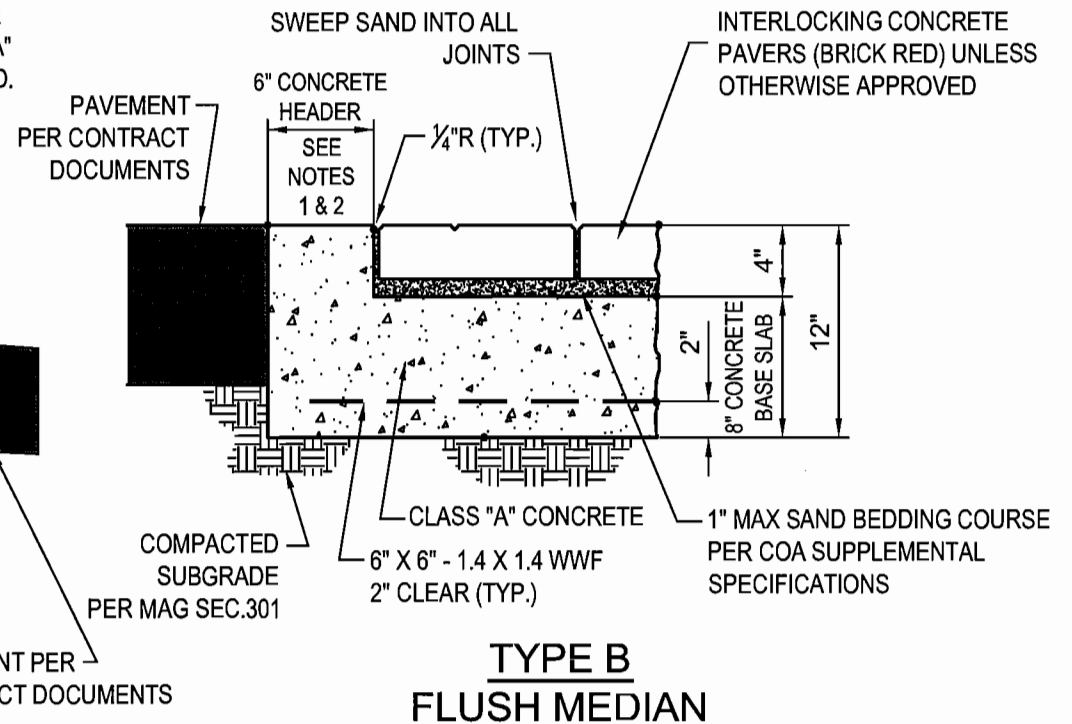
Daniel W. Fitzhugh
DATE: 04-07-08

ACCEPTABLE PATTERNS



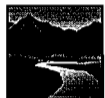
NOTES:

1. $\frac{1}{2}$ " EXPANSION JOINT, ASTM D-1751 PER MAG SEC. 729, EVERY 50'.
2. CONTRACTION JOINTS EVERY 10'.
3. ALL MATERIALS AND CONSTRUCTION PER COA SUPPLEMENTAL SPECIFICATIONS.



DETAIL NO.

A1221

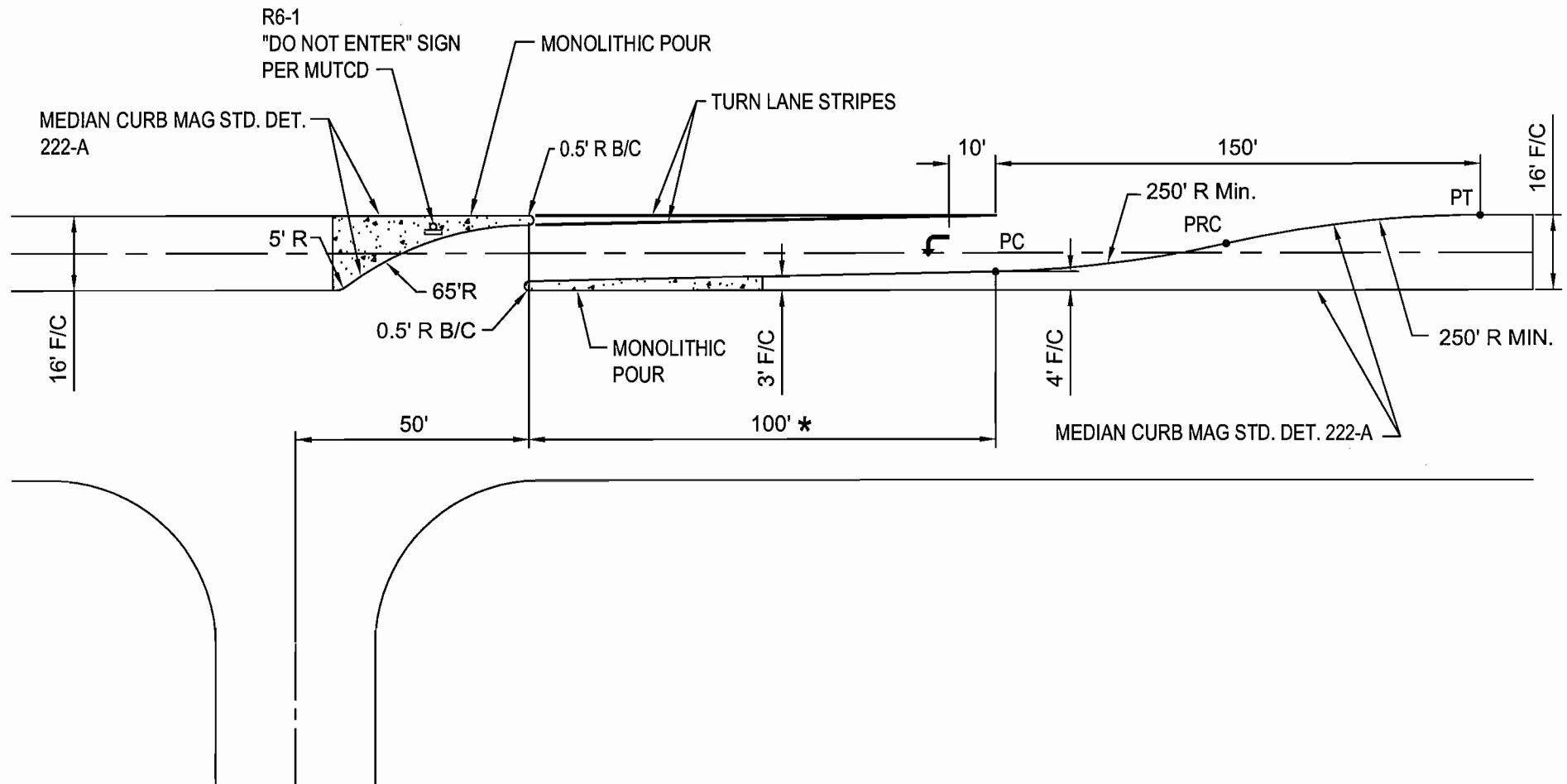


**CITY OF AVONDALE
STANDARD DETAIL**

MEDIAN CONCRETE PAVERS

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08

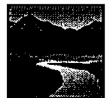


NOTES:

1. ALL DIMENSIONS ARE TO FACE OF CURB OR CENTER OF LANE LINE.
2. ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE IN COMPLIANCE WITH THE LATEST MUTCD, MCDOT, AND COA STANDARDS.

* LONGER STORAGE MAY BE REQUIRED BASED ON TRAFFIC STUDY.

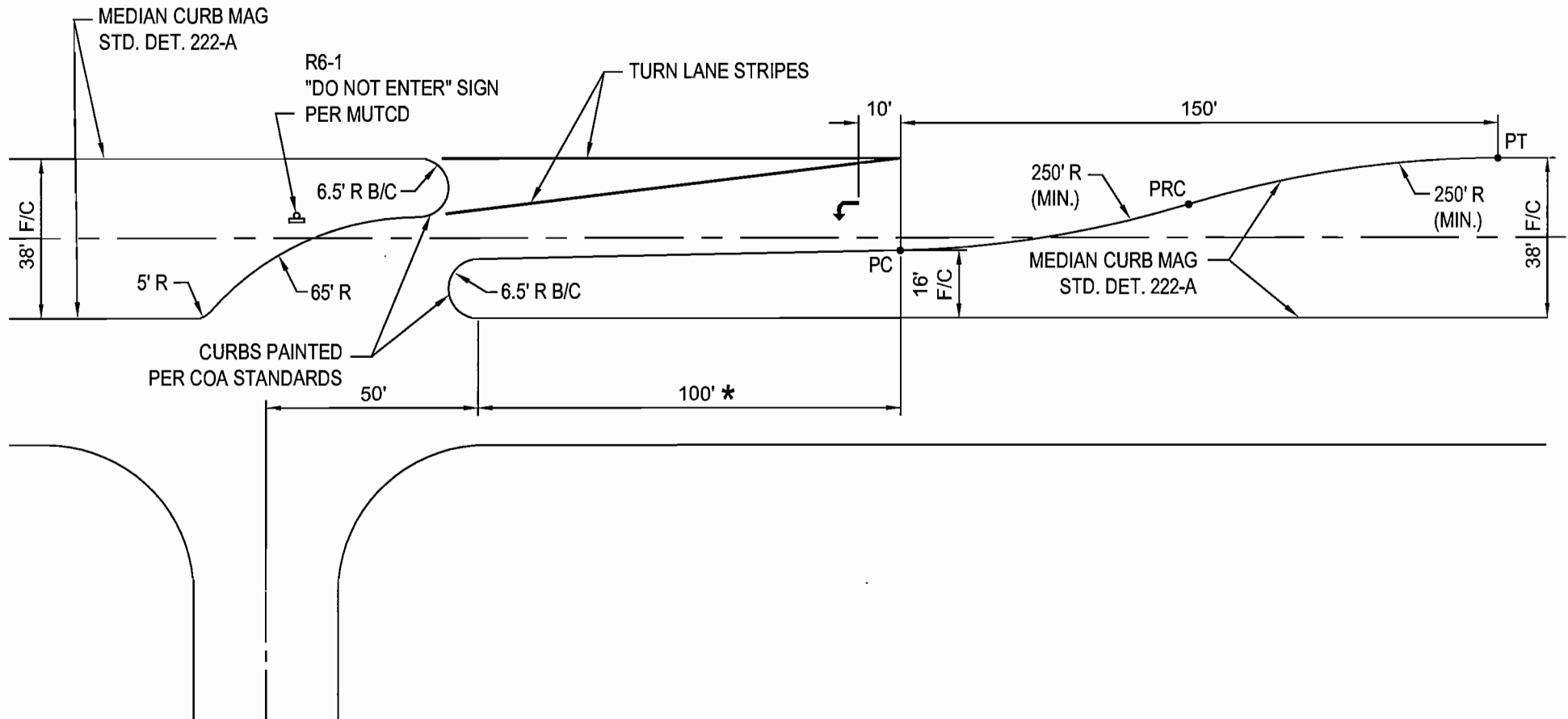
DETAIL NO.
A1222



**CITY OF AVONDALE
STANDARD DETAIL**

**"LEFT IN ONLY" MEDIAN
DESIGN 16' WIDE MEDIAN**

APPROVED BY:
Daniel W. Fitzhugh
DATE: 04-07-08

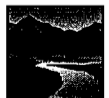


NOTES:

1. ALL DIMENSIONS ARE TO FACE OF CURB OR CENTER OF LANE LINE.
2. ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE IN COMPLIANCE WITH THE LATEST MUTCD, MCDOT, AND COA STANDARDS.

* LONGER STORAGE MAY BE REQUIRED BASED ON TRAFFIC STUDY.

DETAIL NO.
A1223

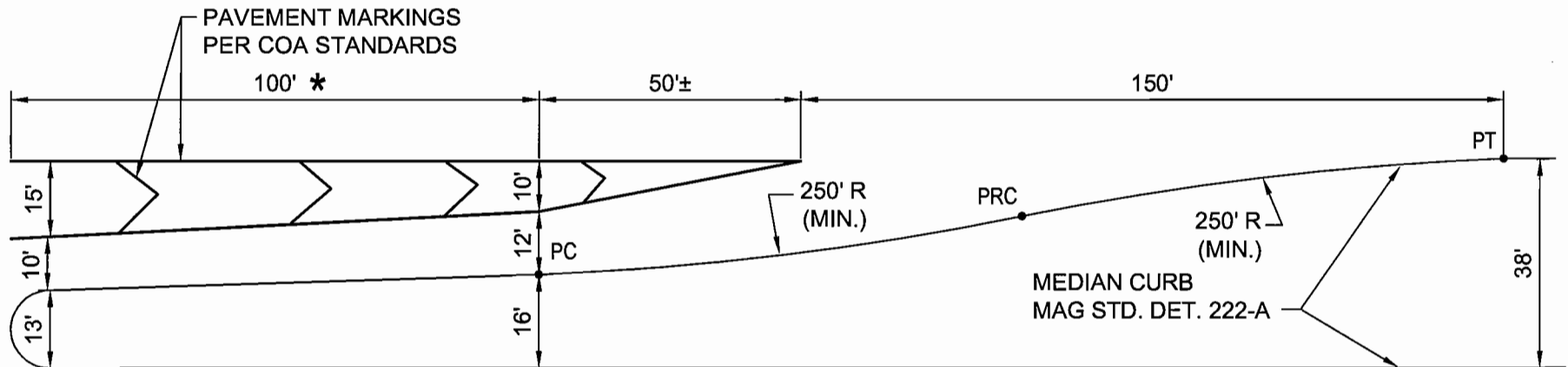


**CITY OF AVONDALE
STANDARD DETAIL**

**"LEFT IN ONLY" MEDIAN
DESIGN 38' WIDE MEDIAN**

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08



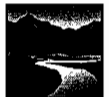
NOTES:

1. ALL DIMENSIONS ARE TO FACE OF CURB OR CENTER OF LANE LINE.
2. ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE IN COMPLIANCE WITH THE LATEST MUTCD, MCDOT, AND COA STANDARDS.

* LONGER STORAGE MAY BE REQUIRED BASED ON TRAFFIC STUDY.

DETAIL NO.

A1224



**CITY OF AVONDALE
STANDARD DETAIL**

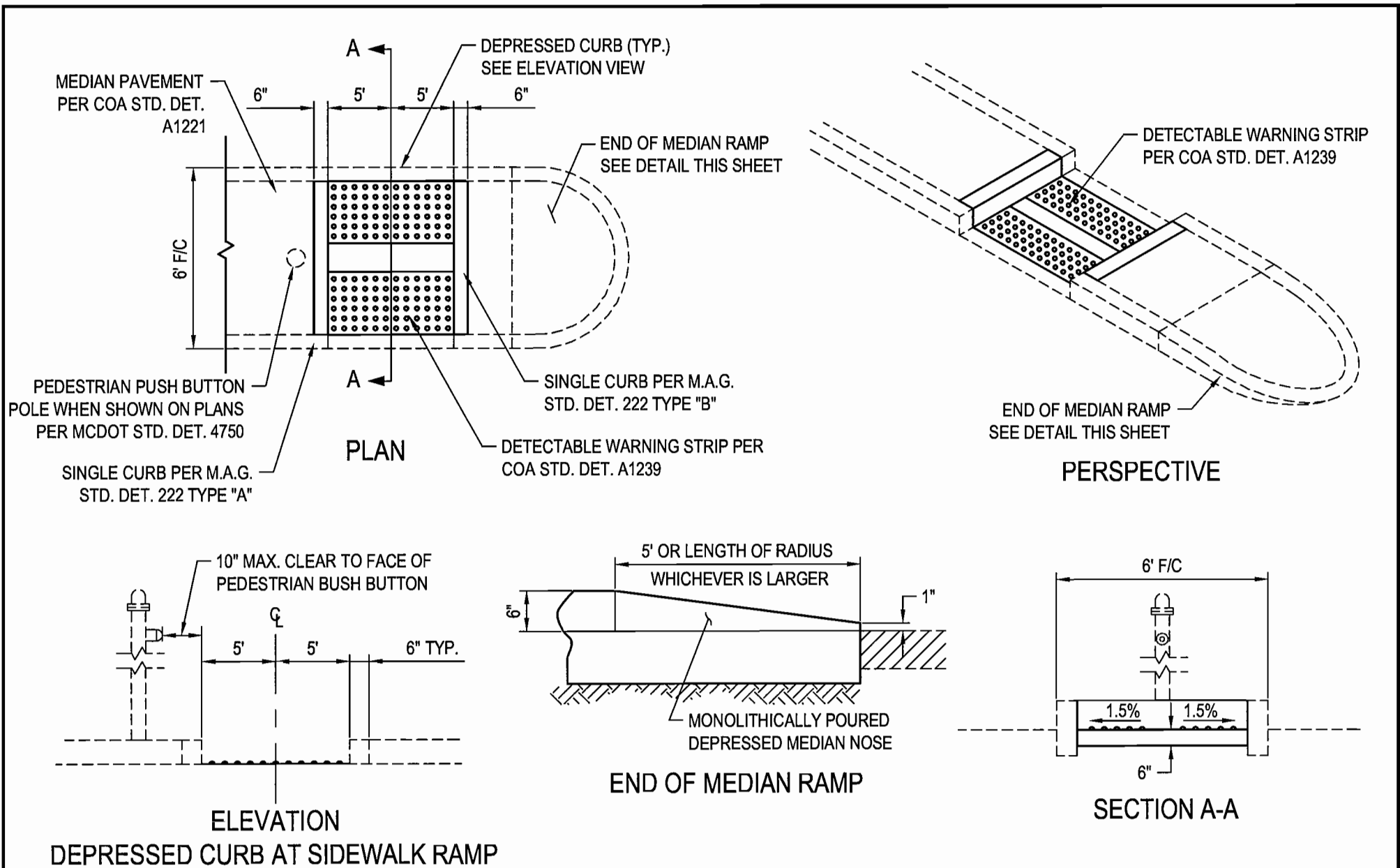
**LEFT TURN LANE
IN 38' MEDIAN DESIGN**

APPROVED BY:

Daniel W. Fitzhugh

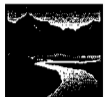
DATE:

04-07-08



DETAIL NO.

A1225



**CITY OF AVONDALE
STANDARD DETAIL**

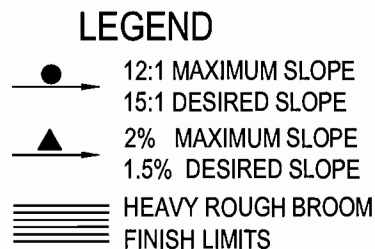
MEDIAN PEDESTRIAN REFUGE

APPROVED BY:

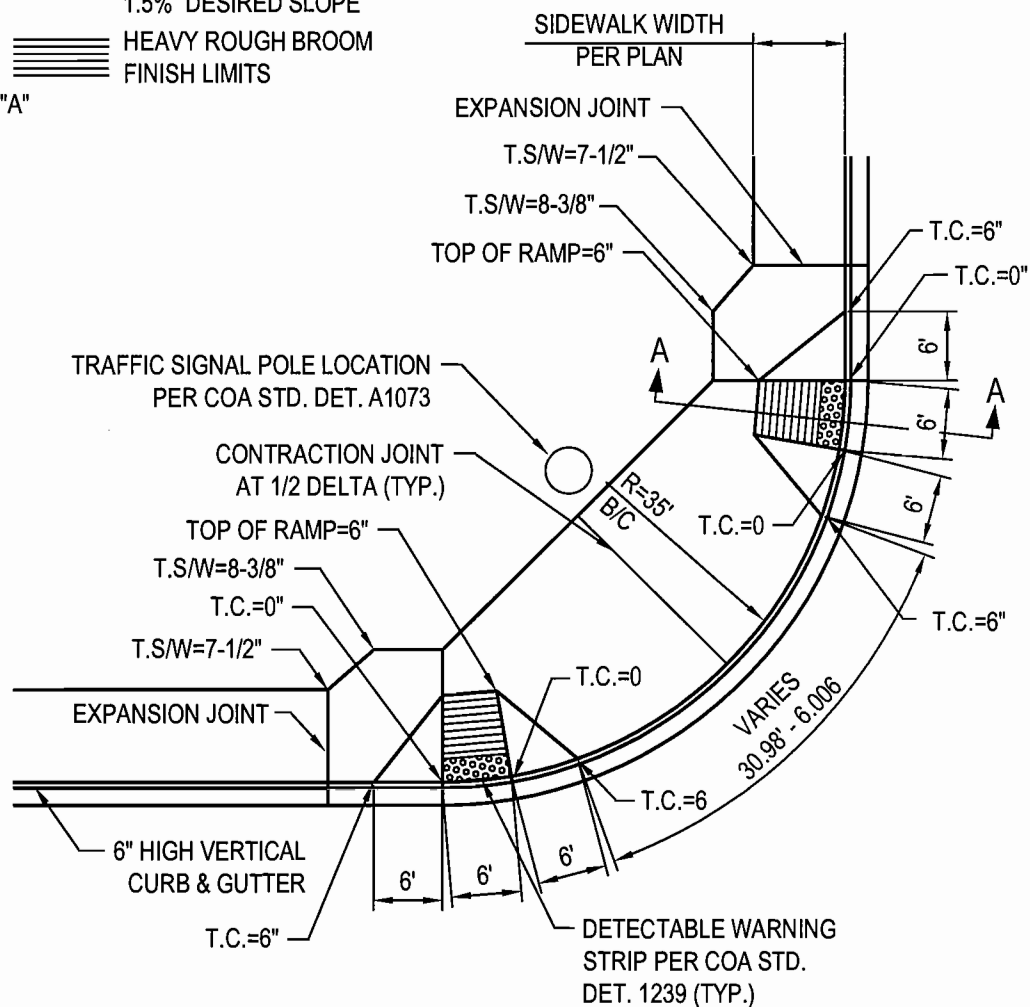
Daniel W. Fitzgibbon

DATE:

04-07-08

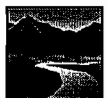


1. CONTROL ELEVATIONS SHOWN ARE IN RELATION TO THE GUTTER AND ARE LOCATED RADially. GUTTER ELEVATION = 0".
2. CONCRETE CURB & GUTTER AT CURB RETURNS WITH RAMPS SHALL BE MAG CLASS A. CONCRETE SIDEWALK AND RAMPS AT CURB RETURNS SHALL BE MAG CLASS A.
3. RAMP CURBS MAY BE POURED MONOLITHIC WITH A CONSTRUCTION JOINT.
4. EXPANSION JOINT FILLER SHALL BE 1/2" BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER ASTM D-1751.
5. USE OF 8'-10' LANDING NEAR TRAILS, MAY BE REQUIRED.
6. IF NECESSARY, THE RAMPS MAY BE ROTATED INWARD TOWARD THE CENTER OF RETURN TO LINE UP WITH OPPOSING RAMPS. HOWEVER ALL ROTATIONS NEED TO OCCUR ABOUT THE CENTER POINT OF THE RETURN SUCH THAT THE RAMP REMAINS PERPENDICULAR TO THE BACK OF CURB.



DETAIL NO.

A1235



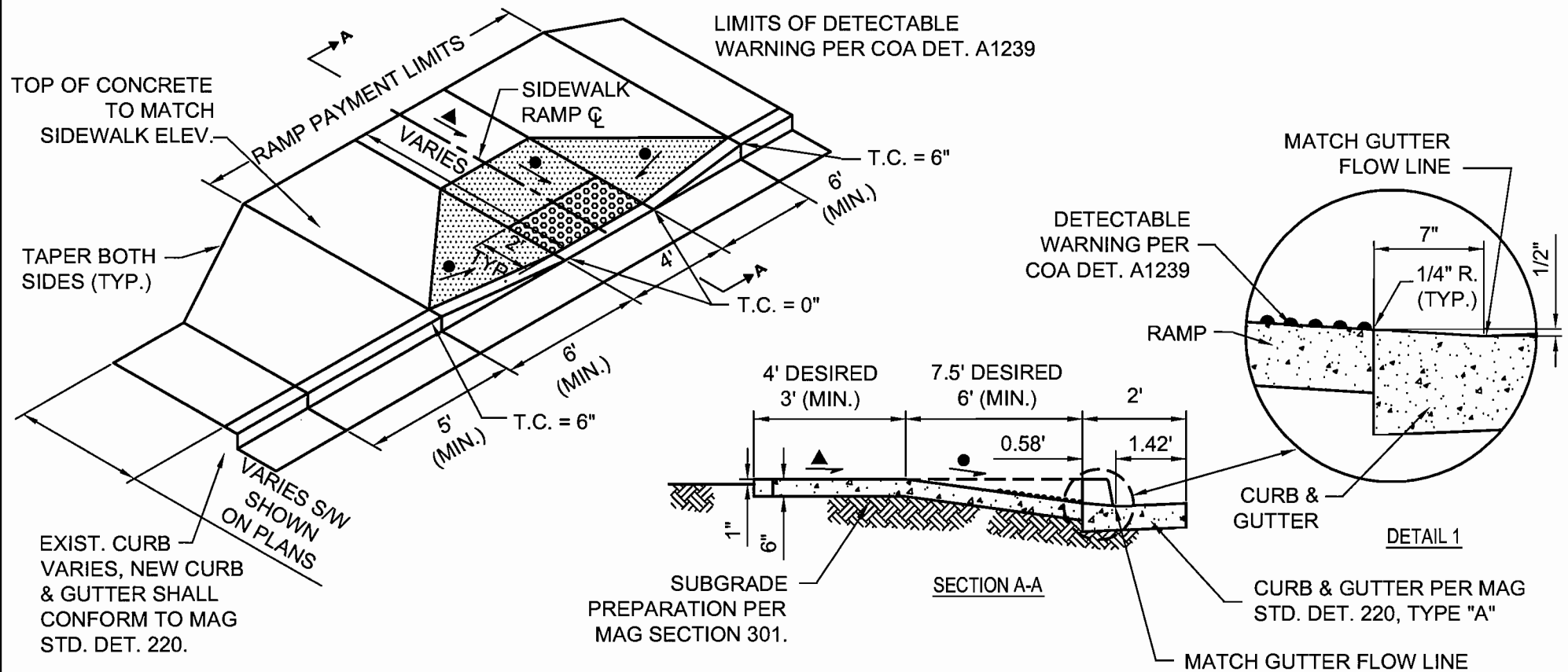
CITY OF AVONDALE
STANDARD DETAIL

DIRECTIONAL RAMPS

APPROVED BY:

DATE:

04-07-08



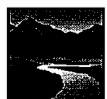
NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTERLINE.

LEGEND

- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- ▲ 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- HEAVY ROUGH BROOM FINISH LIMITS

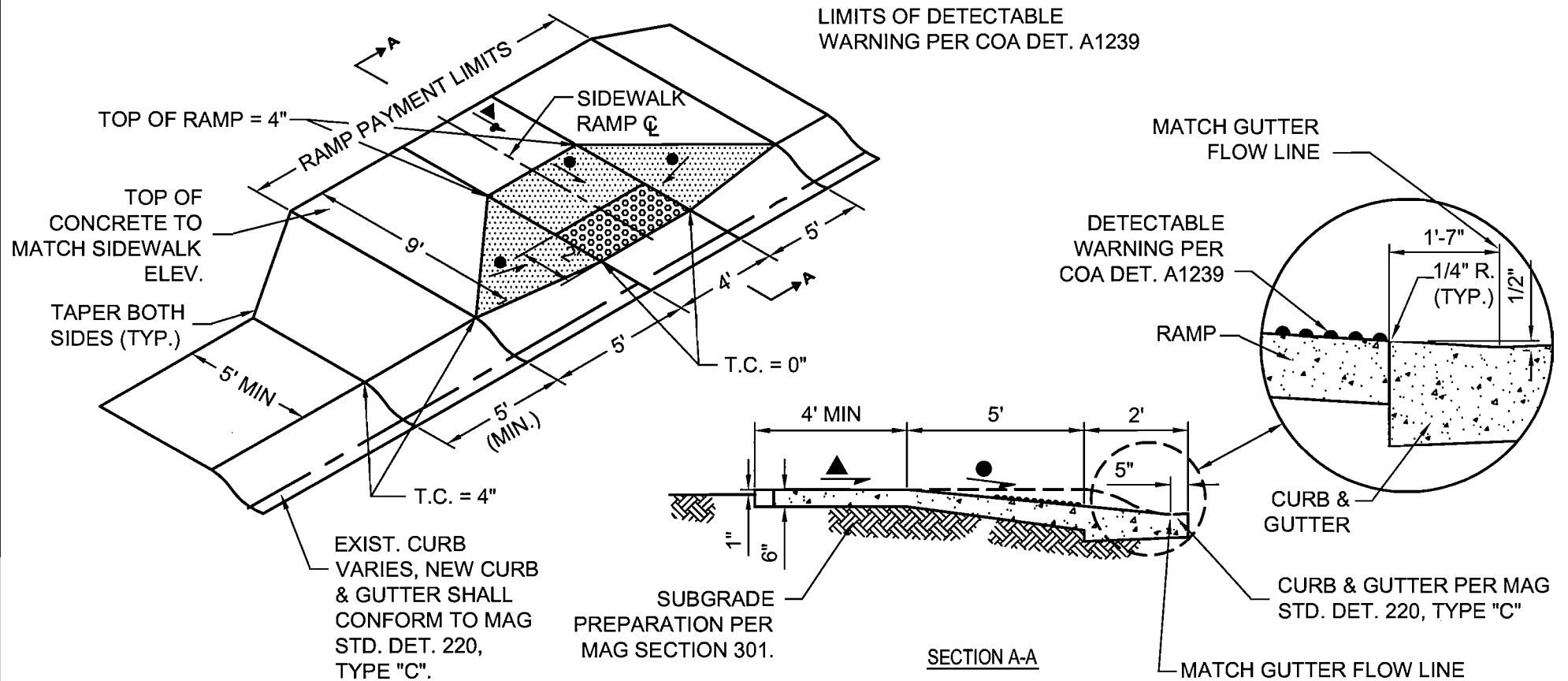
DETAIL NO.
A1237-1



CITY OF AVONDALE
STANDARD DETAIL

MID-BLOCK SIDEWALK RAMP **6" VERTICAL CURB**

APPROVED BY: *Daniel W. Fitzhugh*
DATE: 04-07-08



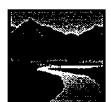
NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTERLINE

LEGEND

- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- ▲ 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- HEAVY ROUGH BROOM FINISH LIMITS

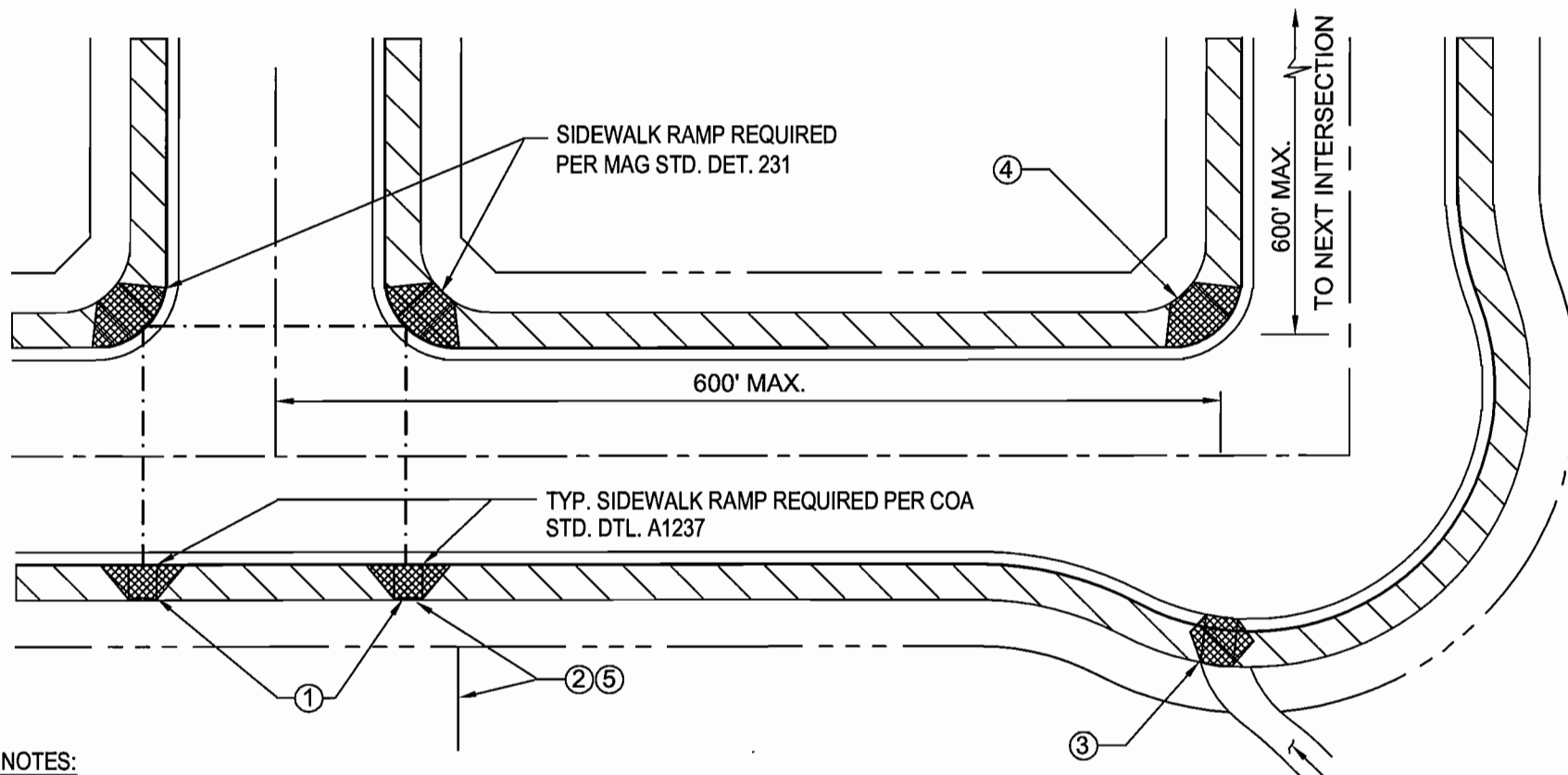
DETAIL NO.
A1237-2



CITY OF AVONDALE
STANDARD DETAIL

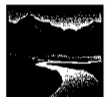
MID-BLOCK SIDEWALK RAMP **ROLL CURB**

APPROVED BY:
Daniel W. Fitzhugh
DATE:
04-07-08



DETAIL NO.

A1238

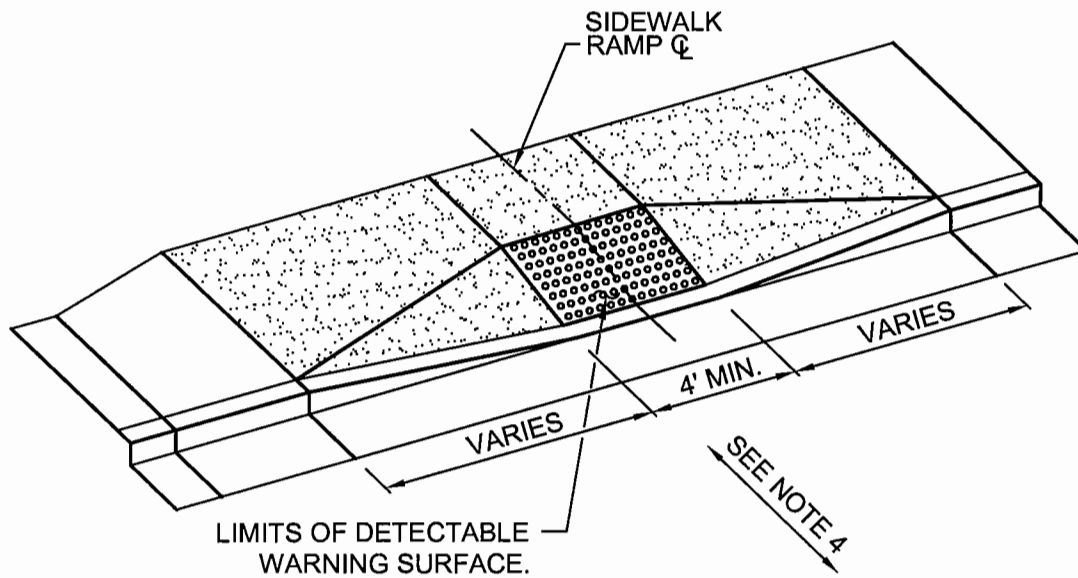


**CITY OF AVONDALE
STANDARD DETAIL**

LOCAL & MINOR COLLECTOR PREFERRED RAMP LOCATIONS

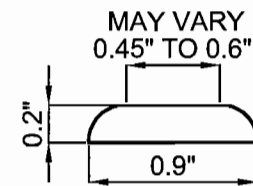
APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08

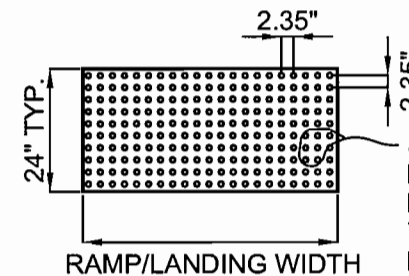


NOTES:

1. ALL DETECTABLE WARNING AREAS SHALL START AT BACK OF CURB, BE 24 INCHES IN DEPTH AND COVER THE COMPLETE WIDTH OF THE RAMP AREA 48 INCHES MINIMUM.
2. 70% VISUAL CONTRAST IS REQUIRED. THE COLOR SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING MATERIAL, AS SPECIFIED ON THE PLANS. COLOR TO BE DETERMINED BY THE CITY STAFF, TERRA-COTTA OR BRICK RED AS THE DEFAULT COLOR.
3. THE SMOOTH AND CLEAN CONCRETE UNDER DETECTABLE WARNING DEVICE AREA SHALL BE INCLUDED IN THE COST OF THE CONCRETE CURB RAMP. THE COST OF FURNISHING AND INSTALLING THE DETECTABLE WARNING DEVICE SHALL BE INCLUDED SEPARATELY AS "DETECTABLE WARNING DEVICE" PER SQUARE FOOT OR AS OUTLINED IN THE SPECIFICATIONS.
4. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
5. RAMP DOME SYSTEMS, TEK-WAY DOME-TILES, NEENAH R-4984 CAST IRON PLATES.
6. MUST BE SET IN WET CONCRETE.



ELEVATION

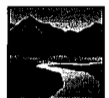


PLAN VIEW

**TRUNCATED DOME
DETECTABLE WARNING**

SHALL BE A SQUARE GRID, EQUAL IN BOTH DIRECTIONS. DOMES SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON OPPOSITE SIDE OF STREET.

DETAIL NO.
A1239

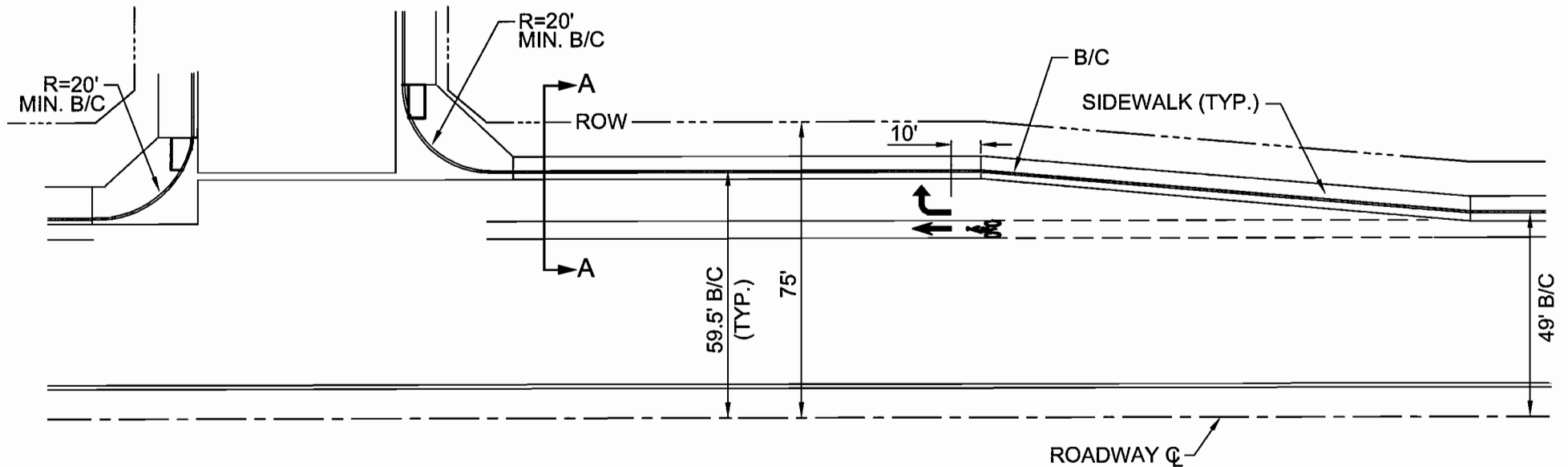
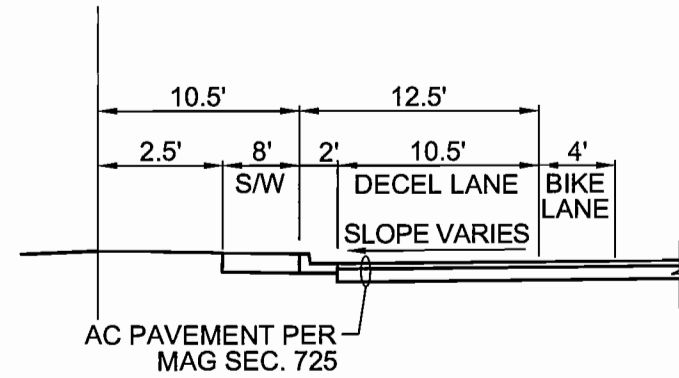


**CITY OF AVONDALE
STANDARD DETAIL**

DETECTABLE WARNING SURFACE

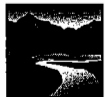
APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08



DETAIL NO.

A1240



**CITY OF AVONDALE
STANDARD DETAIL**

DECELERATION LANE ASPHALT

APPROVED BY:

Daniel W. Fitzhugh

DATE:

04-07-08



DECELERATION LANE CONCRETE

APPROVED BY:

DATE: 04-07-08

STREET CLASSIFICATION	DEVELOPMENT TYPE									
	SINGLE FAMILY RESIDENTIAL				MULTI-FAMILY/ COMMERCIAL		TRUCK FACILITIES		SERVICE STATION	
	SINGLE VEHICLE		MULTIPLE VEHICLE							
	MIN.	MAX.	MIN.	MAX.	MIN.	MAX.	MIN.	MAX.	MIN.	MAX.
LOCAL	14'	18'	(N*10)+4' **	(N*10)+8' **	N/A	N/A	N/A	N/A	N/A	N/A
INDUSTRIAL COLLECTOR	N/A	N/A	N/A	N/A	30'	50' *	40'	80' *	40'	80' *
MINOR COLLECTOR	16'	20'	(N*10)+8' **	(N*10)+12' **	28'	40'	40'	80' *	40'	80' *
MAJOR COLLECTOR	18'	24'	(N*10)+8' **	(N*10)+12' **	30'	60' *	40'	80' *	40'	80' *
PHASED ARTERIAL	N/A	N/A	N/A	N/A	38'	60' *	40'	80' *	40'	80' *
ARTERIAL	N/A	N/A	N/A	N/A	38'	60' *	40'	80' *	40'	80' *

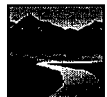
NOTES:

* DRIVEWAYS EXCEEDING 40 FEET IN WIDTH REQUIRE A MEDIAN AND DUAL EGRESS/INGRESS.

** N IS THE NUMBER OF VEHICLES PLANNED TO BE GARAGED

DETAIL NO.

A1250

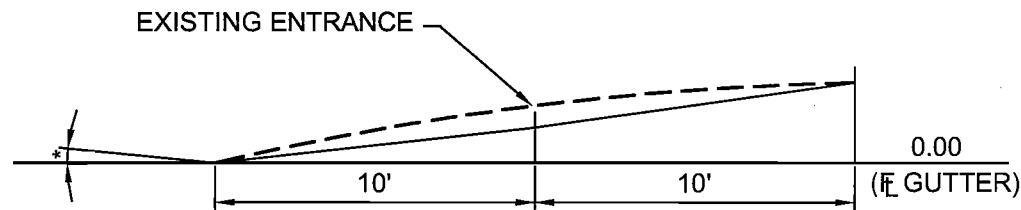
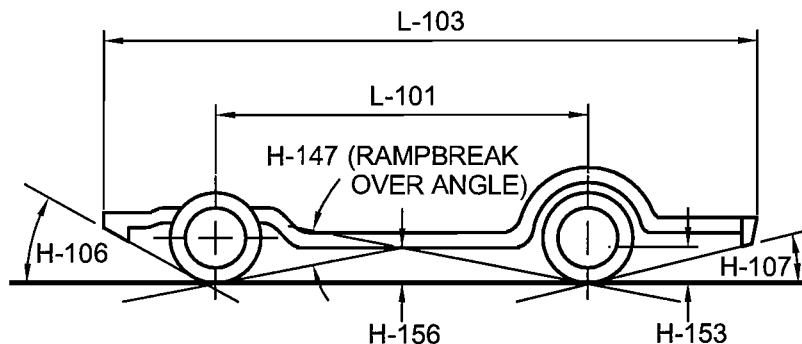


**CITY OF AVONDALE
STANDARD DETAIL**

DRIVEWAY WIDTHS POLICY

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08



* 0.03% MAXIMUM TRANSVERSE SLOPE ALLOWABLE

GROUND CLEARANCE DIMENSIONS:

H-106 - ANGLE OF APPROACH = 8.6 DEGREES
H-107 - ANGLE OF DEPARTURE = 8.3 DEGREES
H-147 - RAMP BREAKOVER ANGLE = 5.53 DEGREES
H-153 - REAR AXLE TO GROUND = 5.5 INCHES
H-156 - MINIMUM GROUND CLEARANCE = 3.1 INCHES
L-101 - WHEELBASE = 9.88 FEET
L-103 - VEHICLE LENGTH = 18.42 FEET

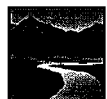
THESE DIMENSIONS ARE FROM THE 1982 MOTOR VEHICLE MANUFACTURERS ASSOCIATION PUBLICATION. COPIES MAY BE OBTAINED FROM TECHNICAL AFFAIRS DIVISION, MOTOR VEHICLE MANUFACTURERS ASSOCIATION, 300 NEW CENTER BUILDING, DETROIT, MICHIGAN 48202.

NOTES:

1. IF THE SUM OF THE STREET CROWN SLOPE, NORMALLY A NEGATIVE SLOPE OF 1.72° (0.03), AND THE POSITIVE SLOPE OF THE DRIVEWAY IS EQUAL TO OR EXCEEDS THE ANGLE OF DEPARTURE, 8.3° (0.146), THE DRIVEWAY MUST BE REDESIGNED TO A POSITIVE SLOPE OF NOT MORE THAN 6° (0.105).
2. ADDITIONAL INCREASES IN THE POSITIVE SLOPE MAY BE MADE AT TEN (10) FOOT INTERVALS. EACH CHANGE MUST BE LESS THAN THE ANGLE OF DEPARTURE, 8.3° (0.146).
3. CHANGES FROM A POSITIVE SLOPE TO A NEGATIVE SLOPE MUST BE LESS THAN THE BREAKOVER ANGLE OF 5.53° (0.097).
4. WHEN MAKING CHANGE FROM A NEGATIVE SLOPE TO A POSITIVE SLOPE, THE SUM OF THE TWO SLOPES MUST BE LESS THAN THE ANGLE OF DEPARTURE, 8.3° (0.146).

DETAIL NO.

A1251



**CITY OF AVONDALE
STANDARD DETAIL**

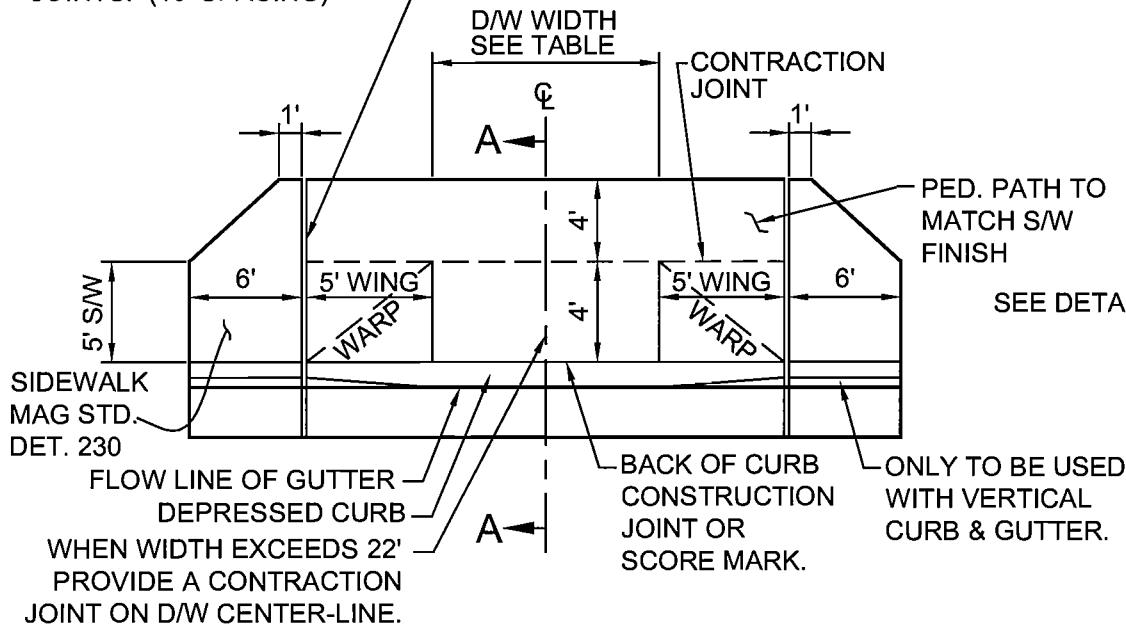
MAXIMUM DRIVEWAY SLOPE

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08

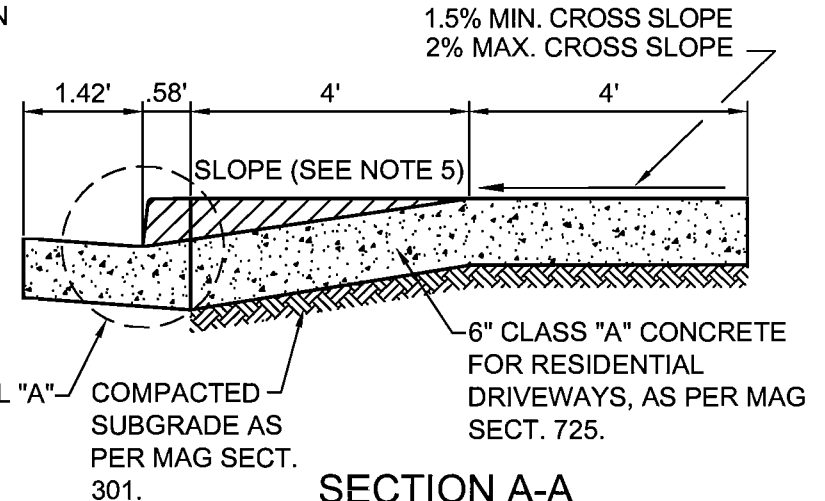
PROVIDE CONSTRUCTION JOINTS TO MATCH CURB JOINTS. (10' SPACING)

FULL DEPTH EXPANSION JOINT THROUGH DRIVEWAY, CURB & GUTTER. EXPANSION JOINT FILLER SHALL BE 1/2" BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER A.S.T.M. D-1751.

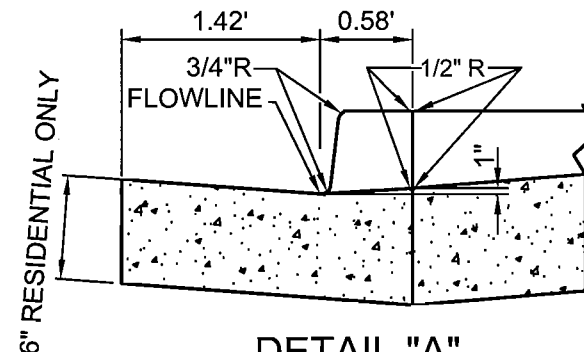


NOTES:

1. THIS DRIVEWAY TO BE USED WITH VERTICAL CURB AND GUTTER ONLY.
2. DEPRESSED CURB SHALL BE PAID FOR AS COMBINED CURB AND GUTTER.
3. PAYMENT FOR DRIVEWAY SHALL BE ON A SQUARE FOOT BASIS.
4. EXPANSION JOINT MATERIAL SHALL BE SECURED IN PLACE PRIOR TO POURING CONCRETE AND SHALL COMPLETELY SEPARATE THE DRIVEWAY SLAB FROM THE SIDEWALK, EXTENDING FROM THE SURFACE TO THE SUBGRADE.
5. WHEN DRIVEWAY IS CONSTRUCTED AT A "T" INTERSECTION AND IS USED AS A RAMP, USE DETAIL A1253



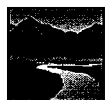
SECTION A-A



DETAIL "A"

REFER TO A1250 FOR DRIVEWAY WIDTHS POLICY

DETAIL NO.
A1252



**CITY OF AVONDALE
STANDARD DETAIL**

**RESIDENTIAL
DRIVEWAY ACCESS**

APPROVED BY:
Daniel W. Fitzhugh
DATE:
04-07-08

PROVIDE CONSTRUCTION JOINTS TO MATCH CURB JOINTS. (10' SPACING)

D/W WIDTH- SEE COA
STD. DET. A1250.

BACK OF CURB-
CONSTRUCTION JOINT
OR SCORE MARK.

CONTRACTION
JOINT

SIZE AS
PER
PLANS

PED. PATH TO
MATCH S/W
FINISH

5' (MIN.)
LANDSCAPED
PARKWAY

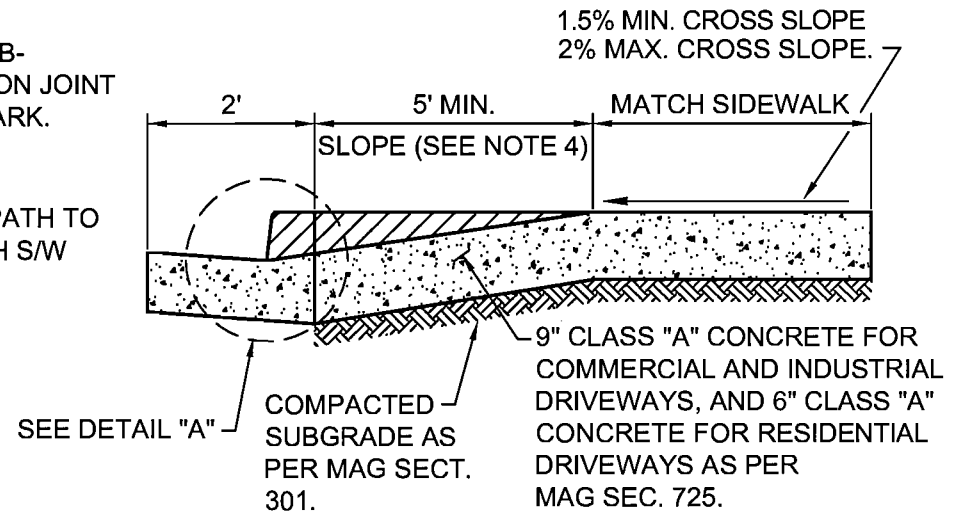
SIDEWALK
MAG STD
DET 230

FLOW LINE OF GUTTER-
DEPRESSED CURB
WHEN WIDTH EXCEEDS 22'
PROVIDE A CONTRACTION
JOINT ON D/W CENTER-LINE.

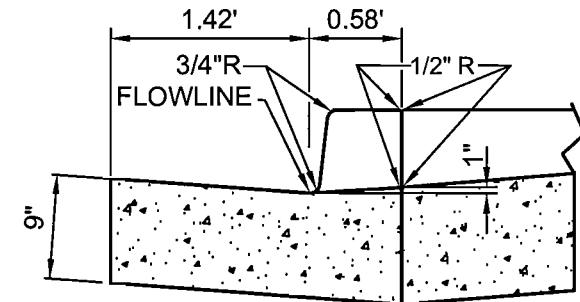
FULL DEPTH EXPANSION JOINT
THROUGH SIDEWALK, CURB &
GUTTER. EXPANSION JOINT FILLER
SHALL BE 1/2" BITUMINOUS TYPE
PRE FORMED EXPANSION JOINT
FILLER A.S.T.M. D-1751.

NOTES:

1. DEPRESSED CURB SHALL BE PAID FOR AS COMBINED CURB AND GUTTER.
2. PAYMENT FOR DRIVEWAY SHALL BE ON A SQUARE FOOT BASIS.
3. EXPANSION JOINT MATERIAL SHALL BE SECURED IN PLACE PRIOR TO POURING CONCRETE AND SHALL COMPLETELY SEPARATE THE DRIVEWAY SLAB FROM THE SIDEWALK, EXTENDING FROM THE SURFACE TO THE SUBGRADE.
4. WHEN DRIVEWAY IS CONSTRUCTED AT A "T" INTERSECTION AND IS USED AS A RAMP, THE SLOPE OF THE DRIVEWAY SHALL BE A MAX OF 12:1.

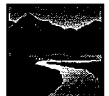


SECTION A-A



DETAIL "A"

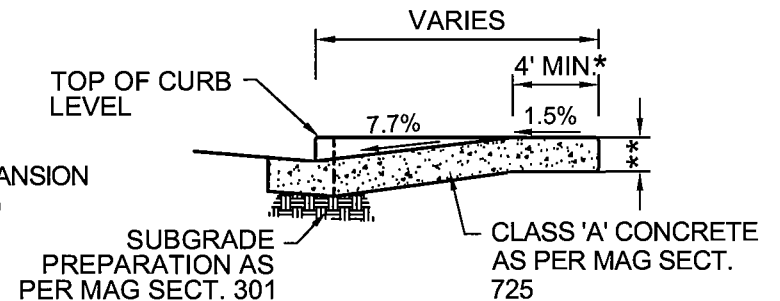
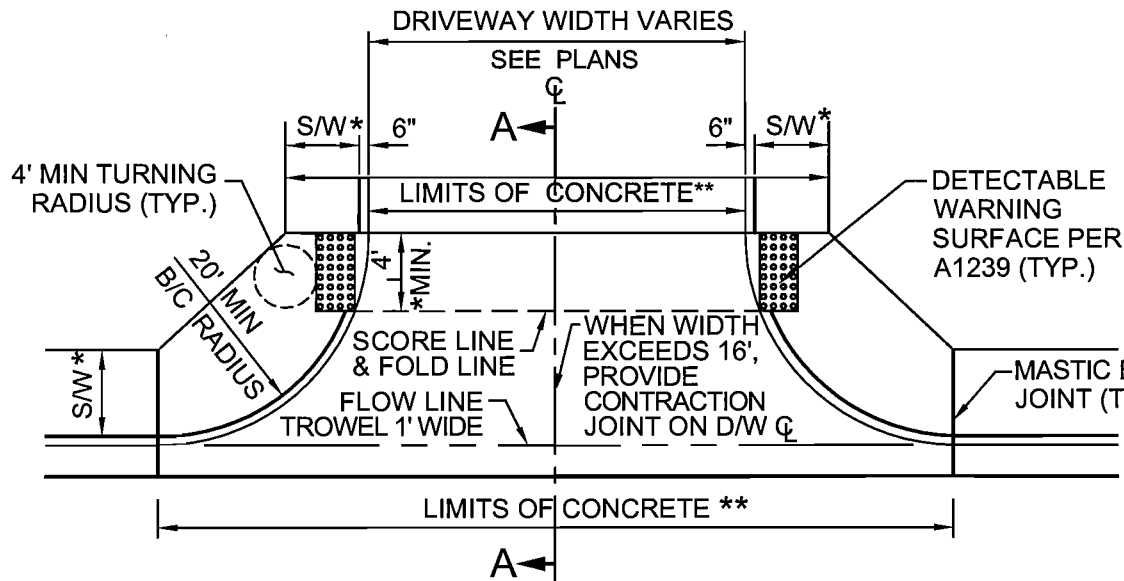
DETAIL NO.
A1252-1



CITY OF AVONDALE
STANDARD DETAIL

RESIDENTIAL DRIVEWAY ACCESS (DETACHED SIDEWALK)

APPROVED BY:
Daniel W. Fitzhugh
DATE:
04-07-08



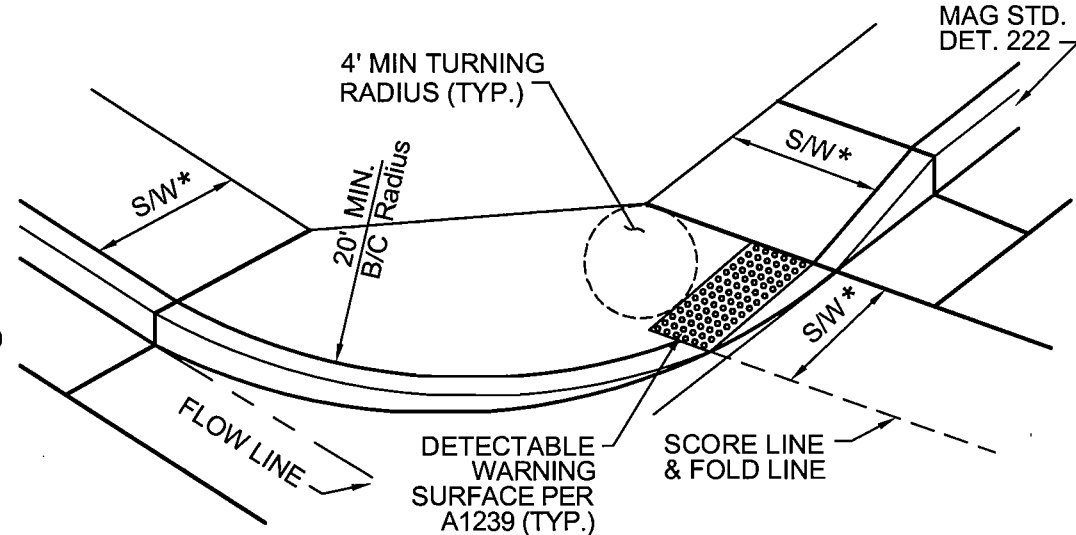
SECTION A-A

NOTES:

EXPANSION JOINT FILLER SHALL BE $\frac{1}{2}$ " BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D-1751.

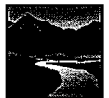
* SIDEWALK WIDTH AS PER PLANS

** 9" THICK CONCRETE FOR COMMERCIAL / INDUSTRIAL OR 6" THICK CONCRETE FOR RESIDENTIAL



DETAIL NO.

A1254

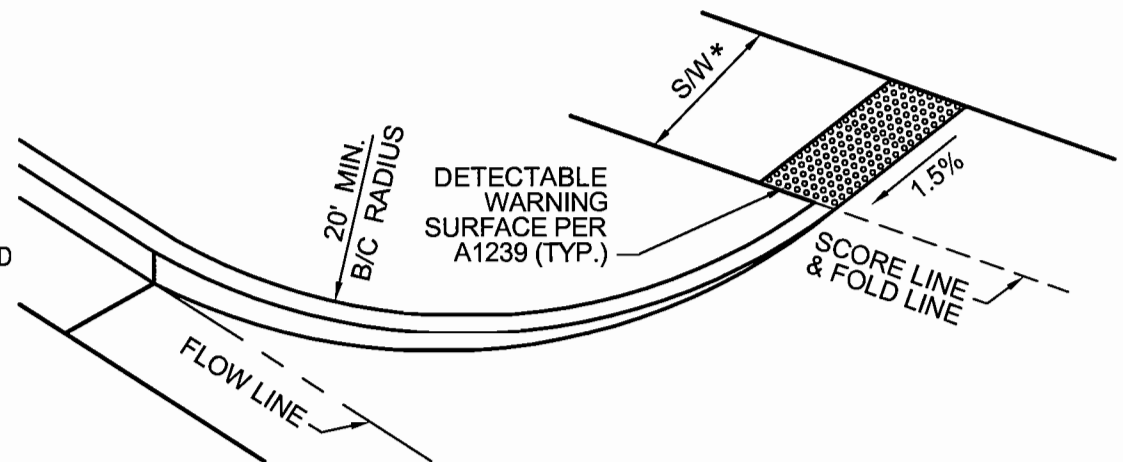
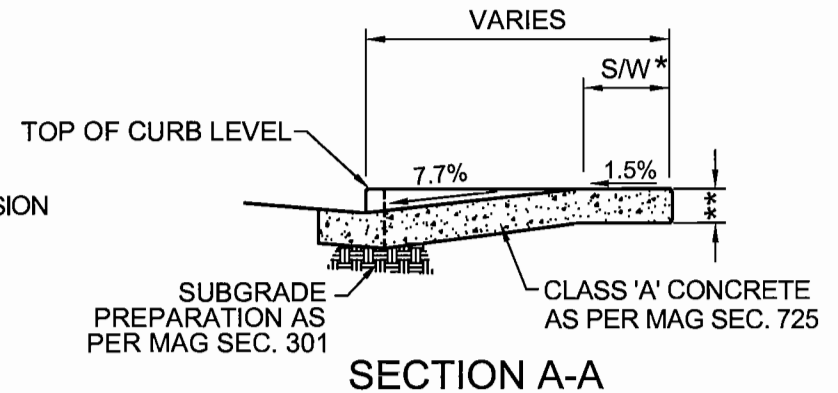
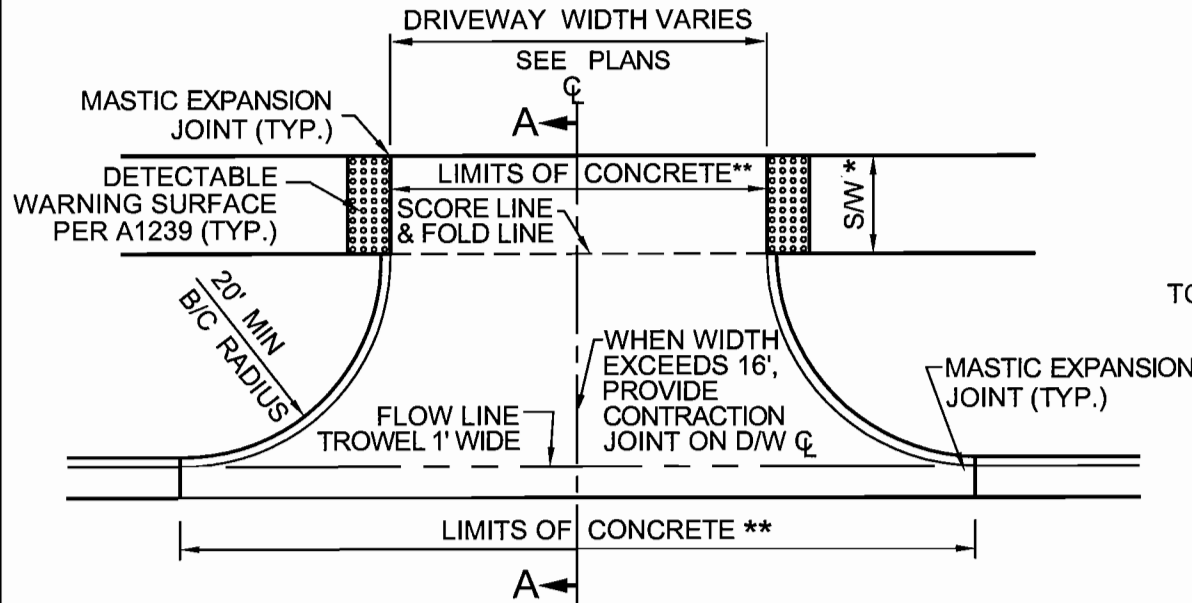


**CITY OF AVONDALE
STANDARD DETAIL**

**MULTI-FAMILY / COMMERCIAL
DRIVEWAY ENTRANCE
(ATTACHED SIDEWALK)**

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08



NOTES:

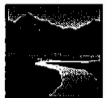
EXPANSION JOINT FILLER SHALL BE $\frac{1}{2}$ " BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D-1751.

* SIDEWALK WIDTH AS PER PLANS

** 9" THICK CONCRETE FOR COMMERCIAL/INDUSTRIAL OR 6" THICK CONCRETE FOR RESIDENTIAL

DETAIL NO.

A1255



**CITY OF AVONDALE
STANDARD DETAIL**

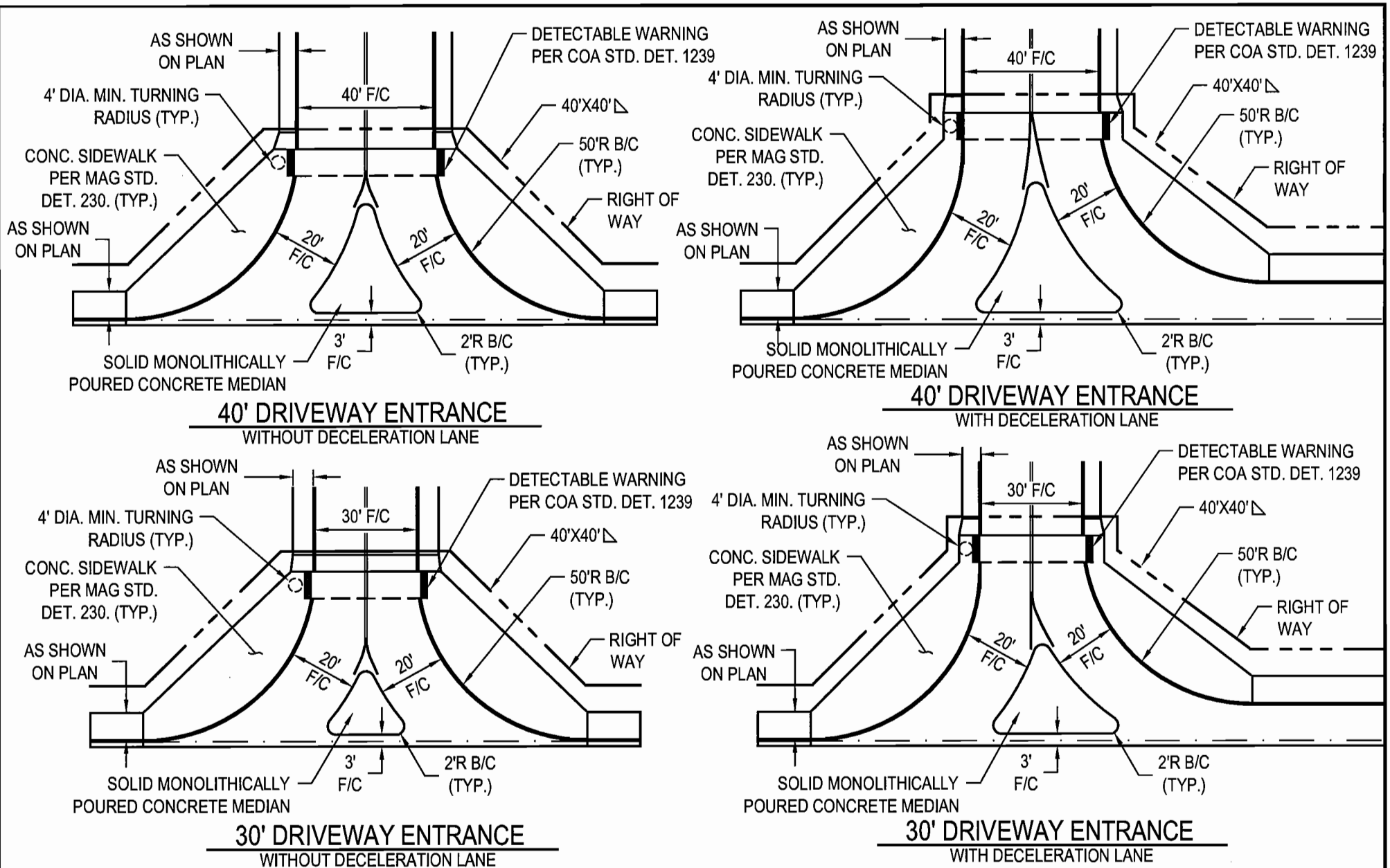
**MULTI-FAMILY / COMMERCIAL
DRIVEWAY ENTRANCE
(DETACHED SIDEWALK)**

APPROVED BY:

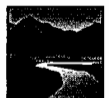
Daniel W. Fitzhugh

DATE:

04-07-08



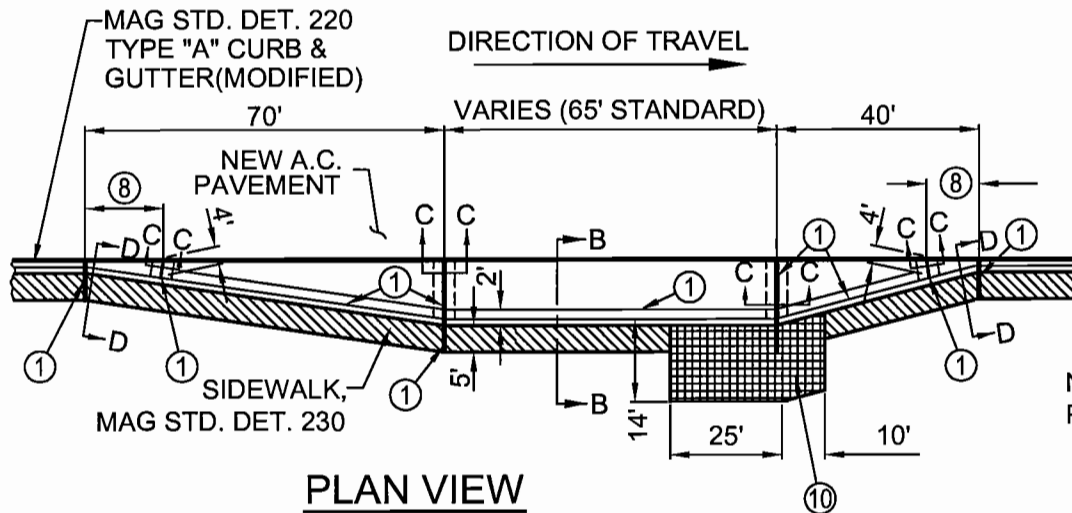
DETAIL NO.
A1256



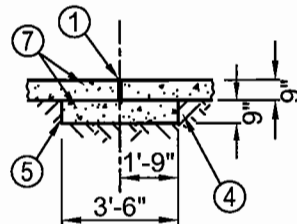
CITY OF AVONDALE
STANDARD DETAIL

RIGHT-IN/RIGHT-OUT **DRIVEWAY ENTRANCE**

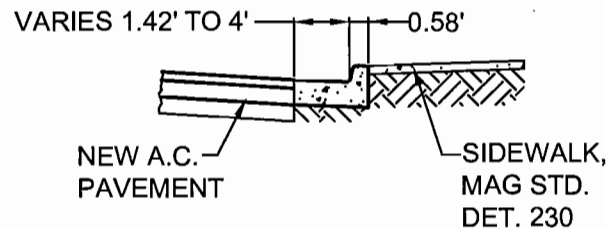
APPROVED BY:
Daniel W. Fitzhugh
DATE:
04-07-08



PLAN VIEW



SECTION C-C



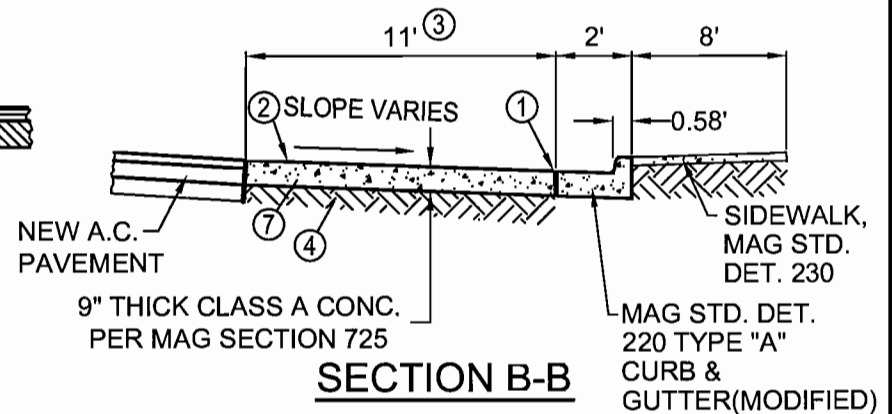
SECTION D-D

NOTES:

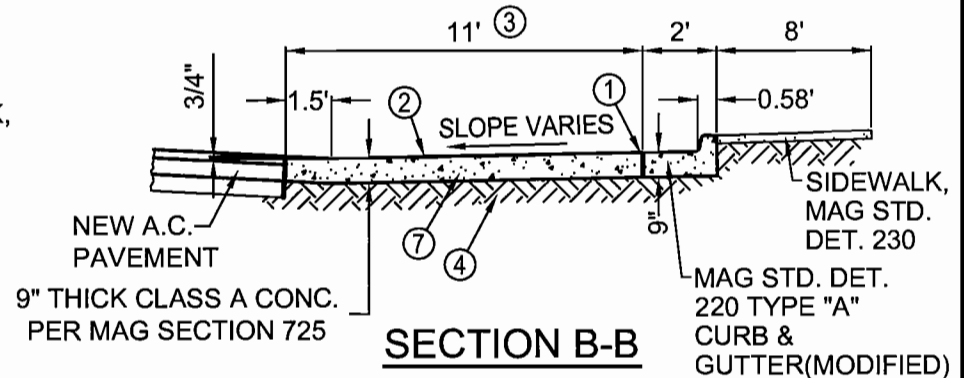
- ① 1/2" BITUMINUS PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D-1751
- ② CONCRETE BUS BAY PAVEMENT SHALL BE BROOM FINISHED.
- ③ MAY BE REDUCED TO 10' MINIMUM IF APPROVED BY CITY.
- ④ SUBGRADE PREPARATION PER MAG SPECIFICATIONS.
- ⑤ CONCRETE PAD TO BE POURED SEPARATELY FROM CONCRETE BUS BAY PAVEMENT.
- ⑥ CONTRACTION JOINTS IN THE BUS BAY PAVEMENT SHALL MATCH THOSE IN THE CURB.

NOTE:

MAINTAIN POSITIVE SLOPE THE LENGTH OF BUS BAY TO PREVENT PONDING.



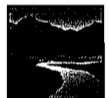
SECTION B-B



SECTION B-B

- ⑦ CONCRETE SHALL BE CLASS "A" PER MAG SPECS. OR CLASS "S", F'C = 3000PSI PER A.D.O.T. SPECS.
- ⑧ CURB & GUTTER-TO-BUS BAY PAVEMENT-TRANSITION (LENGTH VARIES)
- ⑨ DRIVEWAYS SHALL NOT BE LOCATED WITHIN THE SHELTER PAD AREA.
- ⑩ BUS SHELTER PAD, SEE DETAIL A1261

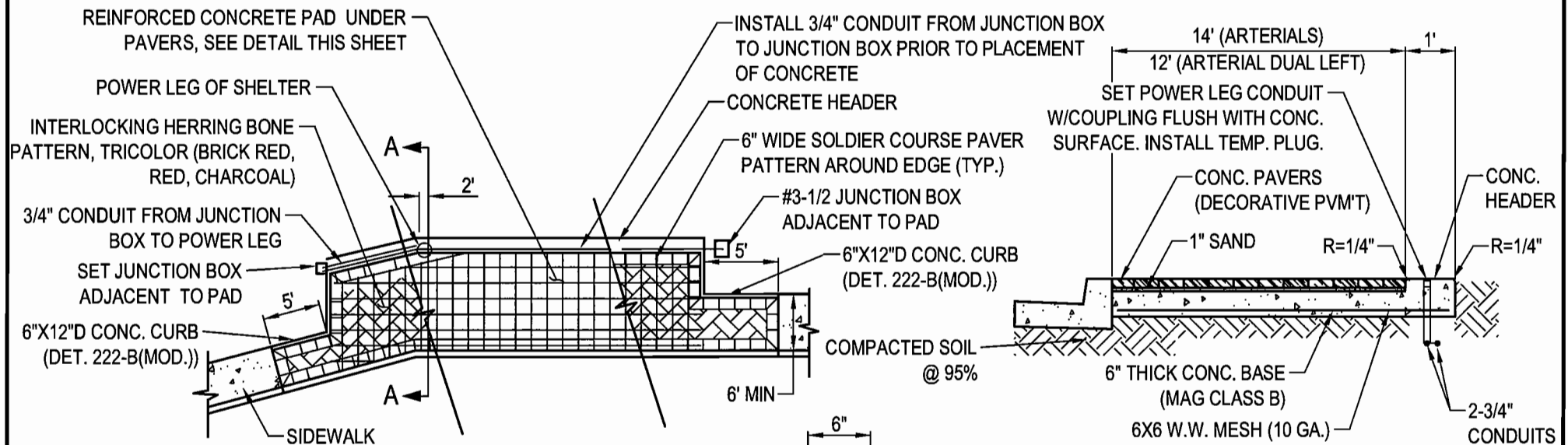
DETAIL NO.
A1260



**CITY OF AVONDALE
STANDARD DETAIL**

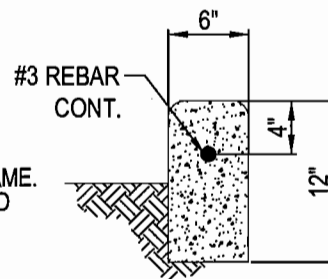
BUS BAY

APPROVED BY:
Daniel W. Fitzhugh
DATE:
04-07-08

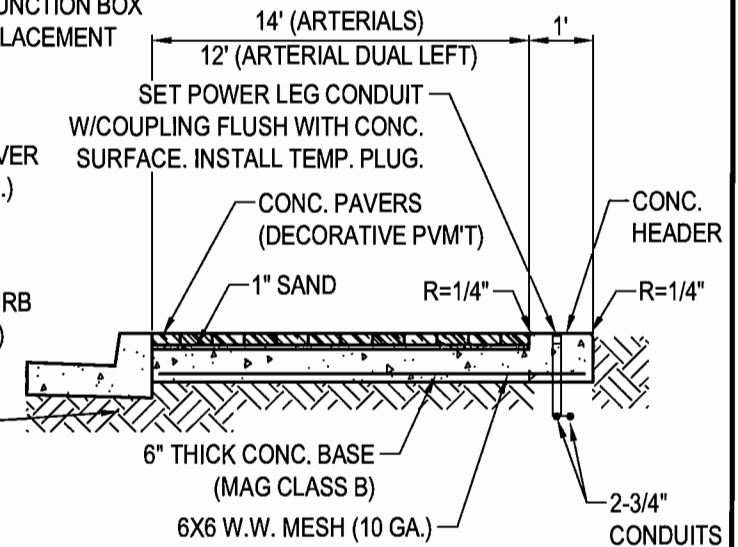


NOTES:

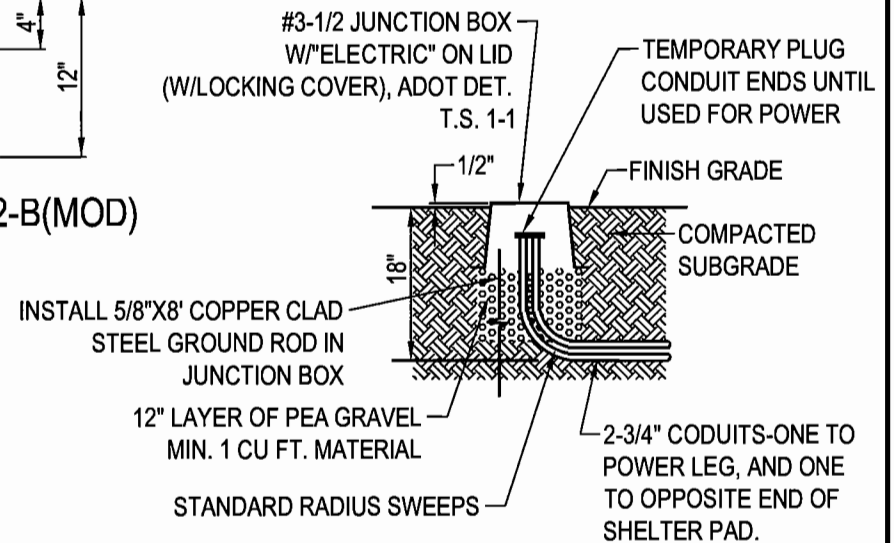
1. ALL CONDUIT SHALL BE P.V.C. SCHEDULE 40, U.L. LISTED.
2. ACTUAL PLAN LAYOUT MAY VARY. ALL DETAIL INFORMATION REMAINS THE SAME. SEE PLANS FOR SPECIFIC LOCATIONS AND DIMENSIONS OF BUS SHELTER PAD CONCRETE BASE AND DECORATIVE PAVEMENT TREATMENT.
3. ANY DECORATIVE PAVEMENT TREATMENT OUTSIDE THE AREA OF THE BUS SHELTER PAD CONCRETE BASE SHALL BE CONSTRUCTED ON A 1" SAND BASE OVER 4-INCHES CEMENT-ENRICHED AGGREGATE BASE SLURRY (1 SACK TYPE II PORTLAND CEMENT PER CUBIC YARD OF AGGREGATE BASE COURSE MATERIAL) OVER 95% COMPACTED SUBGRADE SOIL.
4. ANY SHELTER OR BUS STOP FURNITURE PLACEMENT SHALL BE LOCATED TO PROVIDE A MIN. 9 ft. WIDE CLEAR SPACE BETWEEN BACK OF CURB AND ANY IMPEDIMENT.
5. ALL COSTS ASSOCIATED WITH ELECTRICAL AND RELATED ITEMS SHOWN ON THESE DETAILS (CONDUITS, JUNCTION BOXES, GROUND ROD, ETC.) SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PAY ITEM FOR CONCRETE BUS SHELTER PAD. SOLAR POWER MAY BE USED AS APPROVED BY CITY STAFF.
6. BUS BAY PAVEMENT, DECORATIVE PAVEMENT (INCLUDING CEMENT-ENRICHED A.B.C. SLURRY AND SANDBASE), 6"X12" D CONCRETE CURB, SINGLE CURB, CURB & GUTTER, SIDEWALKS, & DRIVEWAYS ARE SEPARATE PAY ITEMS.



DETAIL 222-B(MOD)



SECTION A-A



SLEEVE SWEEP & JUNCTION BOX DETAIL

DETAIL NO.

A1260-1



**CITY OF AVONDALE
STANDARD DETAIL**

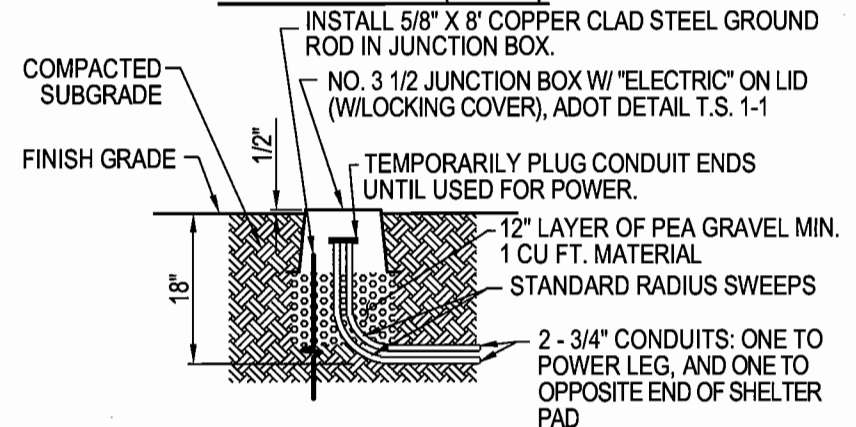
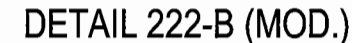
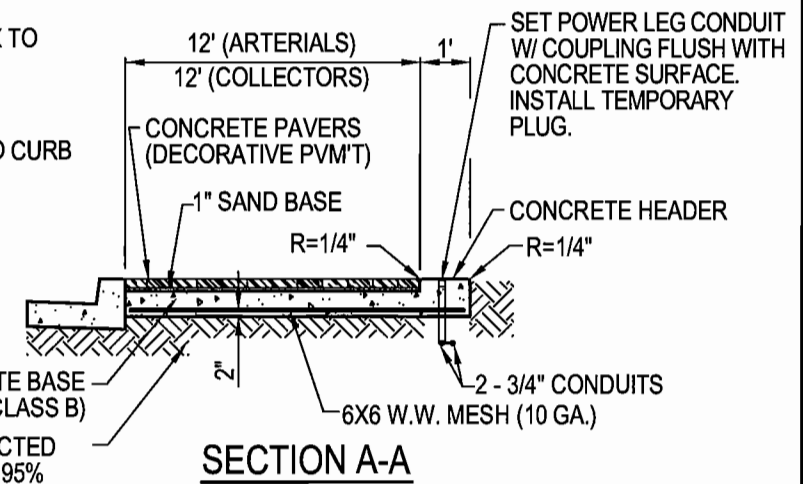
BUS BAY SHELTER

APPROVED BY:

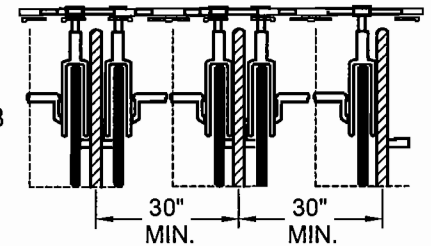
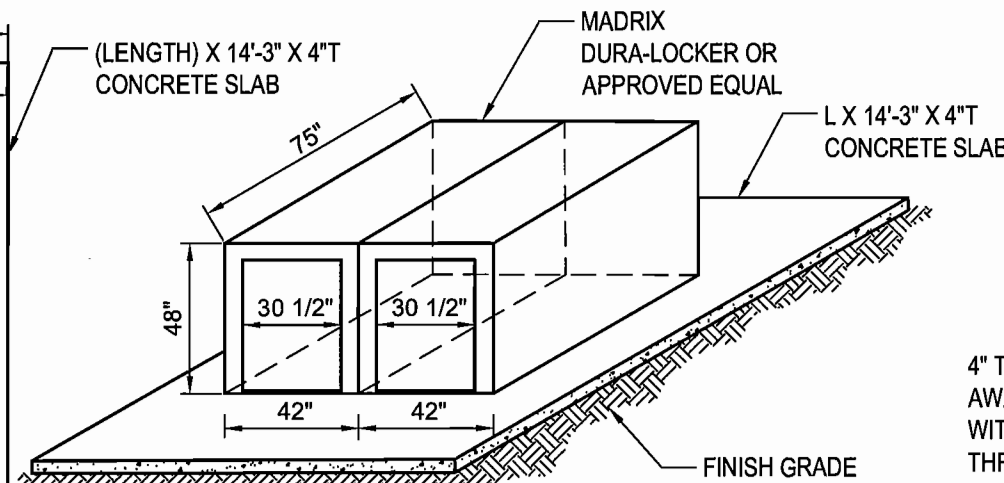
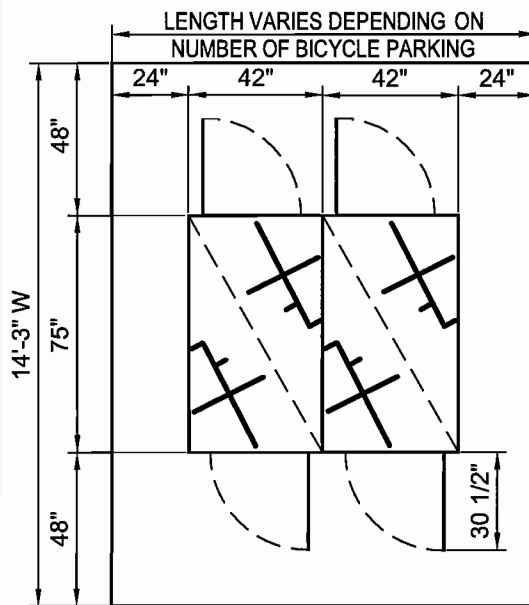
Daniel W. Fitzhugh

DATE:

04-07-08



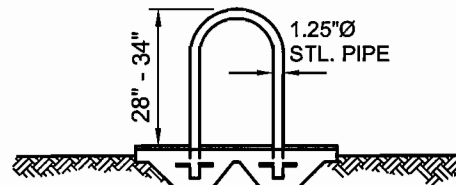
04-07-08



RACK SPACING

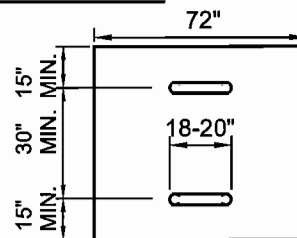
4" THICK CONCRETE SLAB (SLOPE 2% AWAY FROM BUILDING TO DRAIN.) WITH 6 X6 W. 1.4 X1.4 W.W.M. THROUGHOUT & WITH (2) 8"W X 12"Ø MONO. FTG'S @ EACH PIPE SETTING

5/8"Ø HEADED ST "L" STUDS, WELDED ONTO SIDED OF PIPE AT EACH END TO BE EMBEDDED IN CONCRETE. (TYP.)

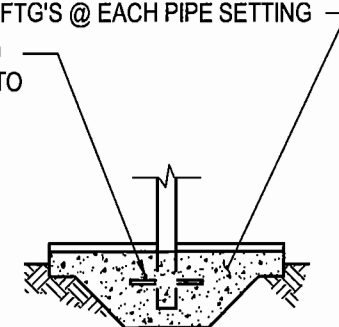


INVERTED "U" RACK

LONG TERM BICYCLE PARKING



SHORT TERM BICYCLE PARKING



RACK MOUNTING

NOTES:

- ALL SHORT TERM BICYCLE PARKING SPACES SHALL INCLUDE A STATIONARY PARKING DEVICE ON A CONCRETE SURFACE WHICH ADEQUATELY SUPPORTS THE BICYCLE AND MUST HOLD AT LEAST 180 DEGREES OF THE WHEEL ARC.
- ALL LONG TERM FACILITIES SHALL CONSIST OF A FULLY ENCLOSED LOCKABLE SPACE ACCESSIBLE ONLY TO OWNER/OPERATOR OF THE BICYCLE, OR ATTENDANT PARKING WITH A CHECK-IN SYSTEM ACCESSIBLE ONLY TO THE ATTENDANT(S), OR A LOCKED ROOM OR OFFICE INSIDE THE BUILDING.
- WHERE NOT SPECIFIED, EITHER SHORT TERM OR LONG TERM PARKING IS PERMISSIBLE.
- EACH SHORT TERM BICYCLE PARKING SPACE SHALL BE A MINIMUM OF 30-INCHES WIDE AND SIX FEET LONG, AND SHALL HAVE A MINIMUM OF EIGHT FEET OF OVERHEAD CLEARANCE.
- ALL REQUIRED PARKING FACILITIES SHALL BE FROM AN APPROVED LIST OF BICYCLE PARKING DEVICES MAINTAINED BY THE MUNICIPAL SERVICES DEPARTMENT. ALTERNATIVE BICYCLE PARKING DEVICES MAY BE USED IF ESTABLISHED AS AN APPROVED EQUIVALENT IN FUNCTION, QUALITY AND CONSTRUCTION.
- FIXED OBJECTS WHICH ARE INTENDED TO SERVE AS BICYCLE PARKING FACILITIES SHALL BE CLEARLY LABELED AS AVAILABLE BICYCLE PARKING.
- BICYCLE PARKING SHALL BE CONSISTENT WITH SURROUNDINGS IN COLOR AND DESIGN AND BE INCORPORATE WHENEVER POSSIBLE INTO BUILDING OR STREET FURNITURE DESIGN.
- BICYCLE PARKING SHALL BE LOCATED AS NEAR THE PRINCIPAL ENTRANCE OF THE BUILDING AS PRACTICABLE.
- BICYCLE PARKING SHALL BE LOCATED IN A CONVENIENT, HIGHLY VISIBLE, ACTIVE, WELL LIGHTED AREA.
- BICYCLE PARKING SHALL BE LOCATED SO AS NOT TO IMPEDE PEDESTRIAN MOVEMENT.
- ALL RACK TYPES ARE 30" O.C.

DETAIL NO.

A1265



**CITY OF AVONDALE
STANDARD DETAIL**

BICYCLE PARKING DETAIL

APPROVED BY:

Daniel W. Fitzhugh
DATE: 04-07-08